

MILITARY STANDARD

MIL-STD-1320-64

TRUCKLOADING

(NAVY)

26 MAY 1993

16" HC PROJECTILE MK 13 AND MK 14 IN PALLET ADAPTER MK 88 MOD 0

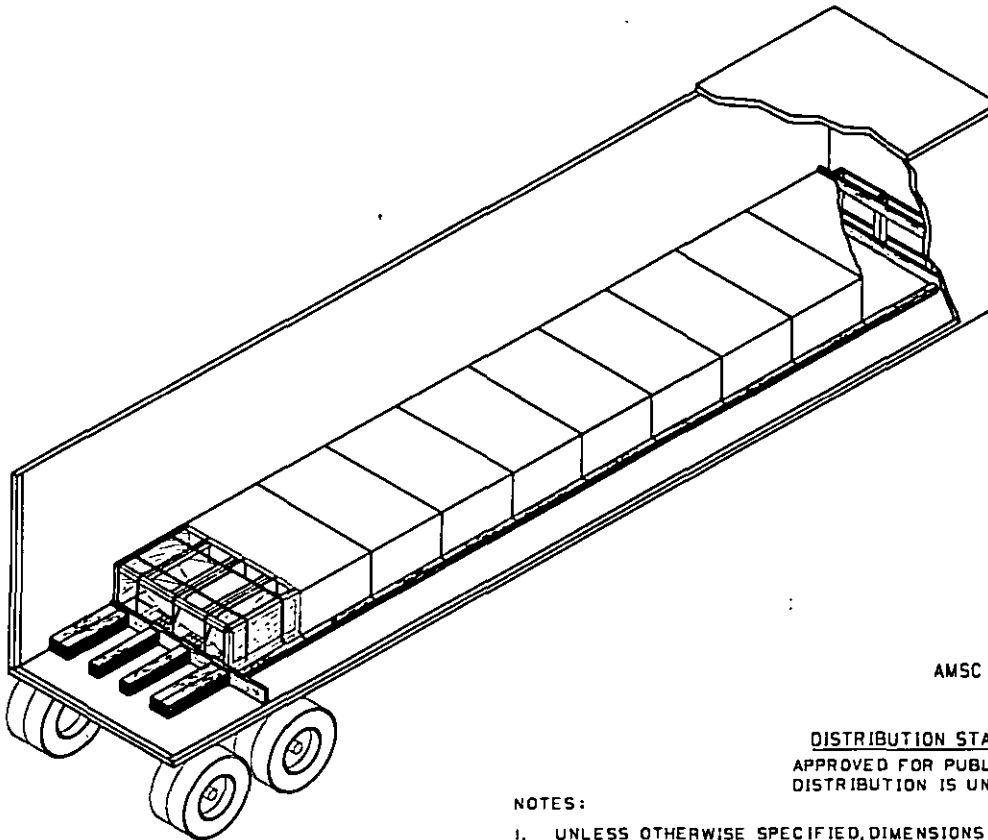
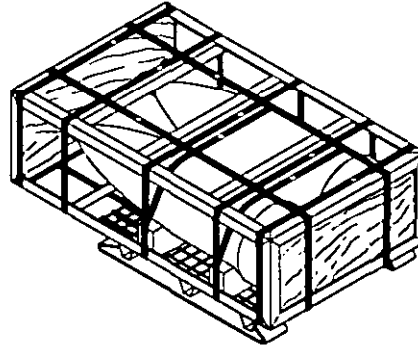
SUPERSEDING
WR-51/64
25 MARCH 1968

UNIT LOAD DATA

UNIT LOAD DOCUMENT.....MIL-STD-1323-149
 WEIGHT OF UNIT LOAD.....4143 LBS Δ
 DIMENSIONS.....69 L X 41 W X 26 H
 CUBE.....42.6 CUFT

Δ DO NOT USE FOR SHIPPING WEIGHT

FOR HAZARD CLASSIFICATION SEE NAVSEA SW020-AC-SAF-010/020/030



AMSC NO. - N/A

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NOTES:

1. UNLESS OTHERWISE SPECIFIED, DIMENSIONS ARE IN INCHES.
2. FOR CROSS REFERENCE TO ASSOCIATED PALLETIZING, CONTAINERLOADING AND CARLOADING MILITARY STANDARDS, REFER TO INDEX TO STANDARDS, MIL-HDBK-236 (NAVY).

| REV LTR | REVISION DESCRIPTION | DATE | TDA | SYS COM |
|---------|----------------------|------|----------|---------|
| | | | APPROVAL | |

FSC 8140

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ORIGINATOR

WPNSTA EARLE, NEW JERSEY

C. Earle 5-26-93
 SIGNATURE TDA, WPNSTA EARLE DATE
[Signature] 5-27-93
 SIGNATURE BY DIRECTION DATE

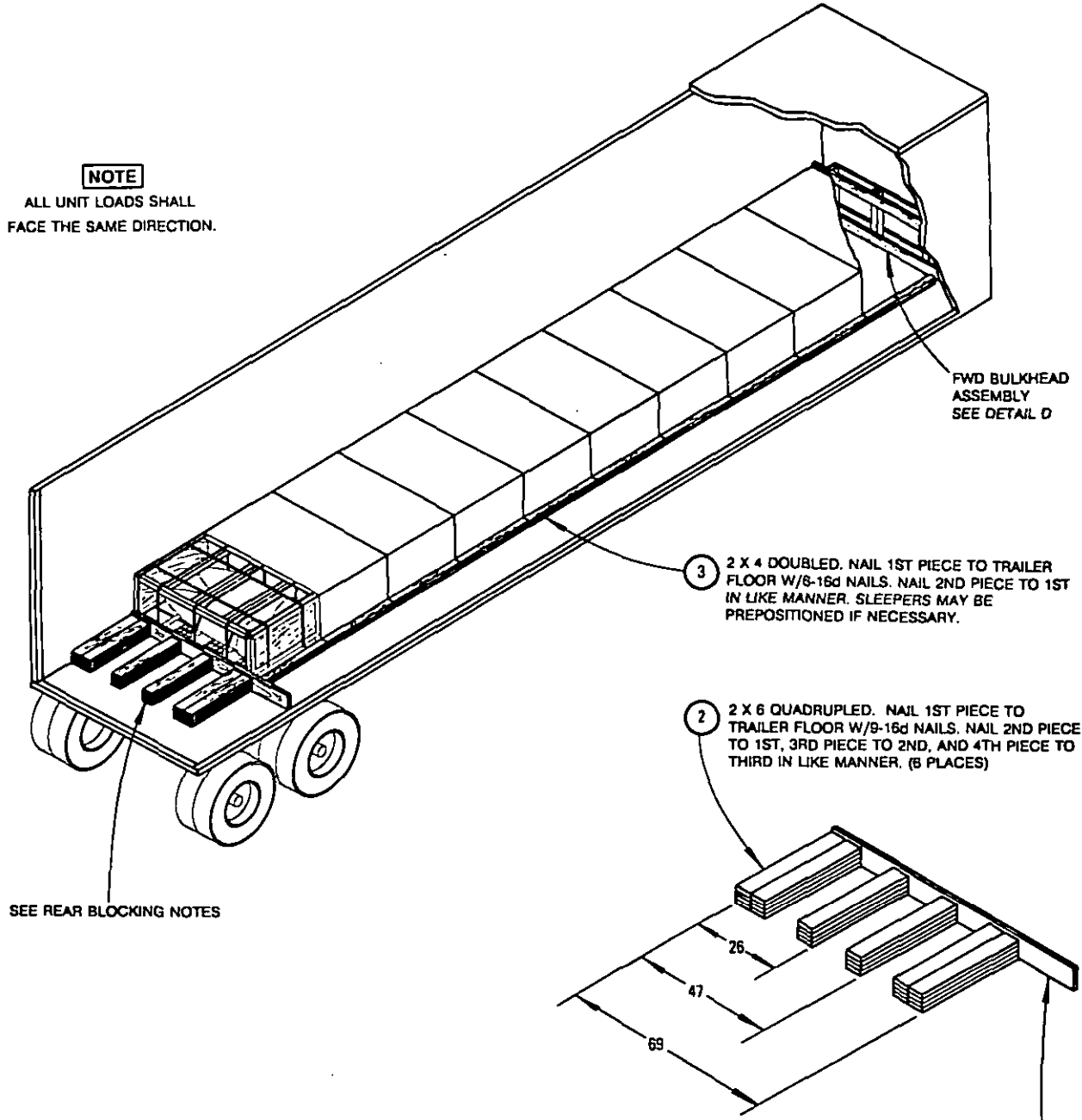
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MIL-STD-1320-64 (NAVY)

GENERAL NOTES:

1. THIS DOCUMENT GIVES DETAILED INSTRUCTIONS FOR TRUCKLOADING FLEET ISSUE UNIT LOADS OF THE 16" HC PROJECTILE MK 13 AND MK 14. THE UNIT LOAD SHALL BE IN ACCORDANCE WITH MIL-STD-1323-149 (NAVY).
2. THE PROCEDURES DESCRIBED HEREIN ARE INTENDED FOR 40 FT AND LONGER ENCLOSED TRAILERS. SHORTER THAN 40 FT TRAILERS MAY BE USED FOR LESS THAN FULL LOADS PROVIDED THE SAME BLOCKING AND BRACING PRINCIPLES OUTLINED IN THIS DOCUMENT CAN BE APPLIED. DO NOT USE TRAILERS WITH ALL METAL FLOORS.
3. THE RECOMMENDED MAXIMUM NUMBER OF UNIT LOADS TO BE LOADED INTO A TRAILER IS TEN UNIT LOADS. ADDITIONAL UNIT LOADS MAY BE ADDED AS LONG AS THE ALLOWABLE GROSS AXLE WEIGHTS ARE NOT EXCEEDED. FOR HEAVIER TRAILERS, LESS THAN TEN UNIT LOADS MAY BE THE MAXIMUM. SEE GENERAL NOTE 7.
4. STACKING OF UNIT LOADS IS NOT AUTHORIZED FOR TRUCKLOADING.
5. FOR LESS THAN FULL TRUCKLOADS (LTL), FOLLOW THE PRINCIPLES CONTAINED IN THIS DOCUMENT, DECREASING THE NUMBER OF UNIT LOADS AS REQUIRED.
6. UNLESS OTHERWISE SPECIFIED, DUNNAGE LUMBER, STEEL STRAPPING, STRAP SEALS, AND NAILS SHALL BE AS SPECIFIED IN THE GENERAL TRUCKLOADING DOCUMENT MIL-STD-1320 (NAVY).
7. THE MAXIMUM GROSS WEIGHT OF THE TRACTOR-TRAILER AND THE ALLOWABLE AXLE WEIGHTS ARE THE RESPONSIBILITY OF THE CARRIER. THE CARRIER WILL ADVISE THE SHIPPER OF THESE LIMITATIONS, AND THE SHIPPER SHALL LOAD THE TRAILER IN SUCH A MANNER THAT THE TRACTOR-TRAILER WILL NOT EXCEED THESE LIMITATIONS.
8. AFTER THE BLOCKING AND BRACING HAS BEEN INSPECTED, ATTACH THE SHIPPING DOCUMENTS TO AN ACCESSIBLE AREA INSIDE THE TRAILER. ATTACH THE PROPER EXPLOSIVES PLACARDS TO BOTH SIDES, FRONT, AND BACK OF THE TRAILER.
9. FOR GENERAL TRUCKLOADING PROCEDURES REFER TO THE GENERAL TRUCKLOADING DOCUMENT, MIL-STD-1320 (NAVY).

NOTE
ALL UNIT LOADS SHALL
FACE THE SAME DIRECTION.



3 2 X 4 DOUBLED. NAIL 1ST PIECE TO TRAILER FLOOR W/8-16d NAILS. NAIL 2ND PIECE TO 1ST IN LIKE MANNER. SLEEPERS MAY BE PREPOSITIONED IF NECESSARY.

2 2 X 6 QUADRUPLED. NAIL 1ST PIECE TO TRAILER FLOOR W/9-16d NAILS. NAIL 2ND PIECE TO 1ST, 3RD PIECE TO 2ND, AND 4TH PIECE TO THIRD IN LIKE MANNER. (8 PLACES)

SEE REAR BLOCKING NOTES

1 CROSSMEMBER SHALL BE SECURED IN POSITION BY TOENAILING ONE 10d NAIL THROUGH EACH BACKUP CLEAT AND INTO THE CROSSMEMBER.

DETAIL **A**

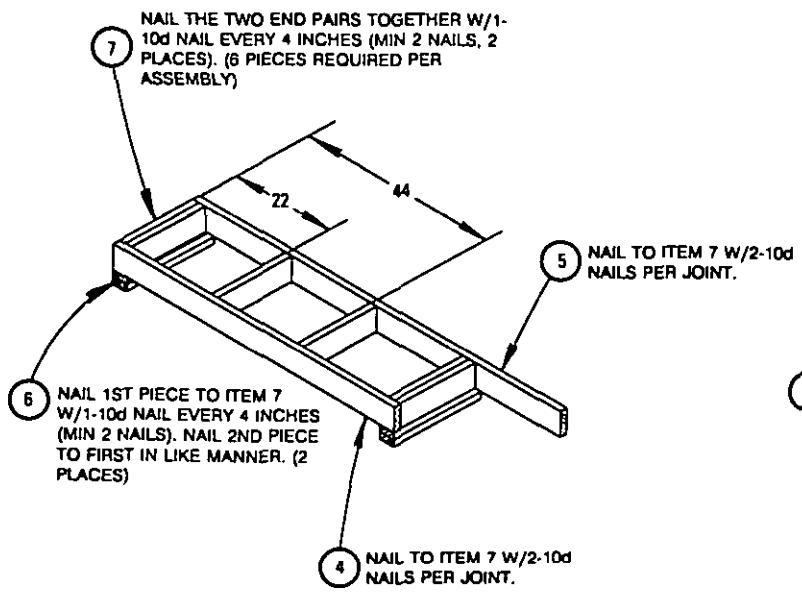
REAR BLOCKING NOTES:

1. WHEN THE SPACE BETWEEN THE AFT-MOST UNIT LOAD AND THE TRAILER DOORS IS GREATER THAN 36 INCHES, USE THE REAR BLOCKING ASSEMBLY SHOWN IN DETAIL A.
2. WHEN THE SPACE BETWEEN THE AFT-MOST UNIT LOAD AND THE TRAILER DOORS IS BETWEEN 12 AND 36 INCHES, USE THE REAR FILLER ASSEMBLY SHOWN IN DETAIL B. THE ASSEMBLY SHALL COMPLETELY FILL THE VOID SPACE.
3. WHEN THE SPACE BETWEEN THE AFT-MOST UNIT LOAD AND THE TRAILER DOORS IS LESS THAN 12 INCHES, USE THE REAR FILLER ASSEMBLY SHOWN IN DETAIL C. THE ASSEMBLY SHALL COMPLETELY FILL THE VOID SPACE.

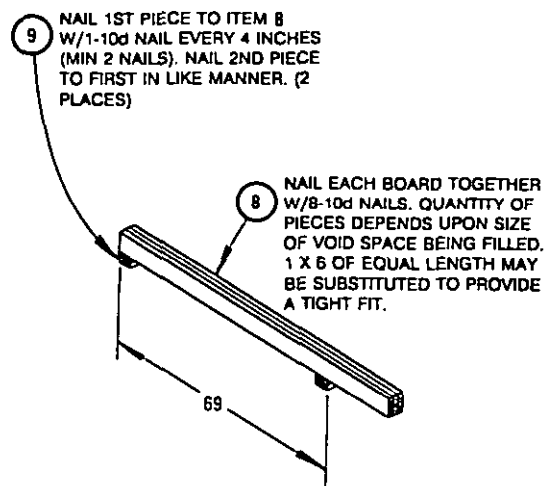
* TW-1" MEANS INSIDE TRAILER WIDTH MINUS ONE INCH.

| | | | |
|------|--------------|----------------|-------------|
| 3 | SLEEPER | WOOD | 2 X 4 X 41 |
| 2 | BACKUP CLEAT | WOOD, DETAIL A | 2 X 6 X 36 |
| 1 | CROSSMEMBER | WOOD, DETAIL A | 2 X 8 TW-1" |
| ITEM | DESCRIPTION | MAT'L/DWG | DIMENSIONS |

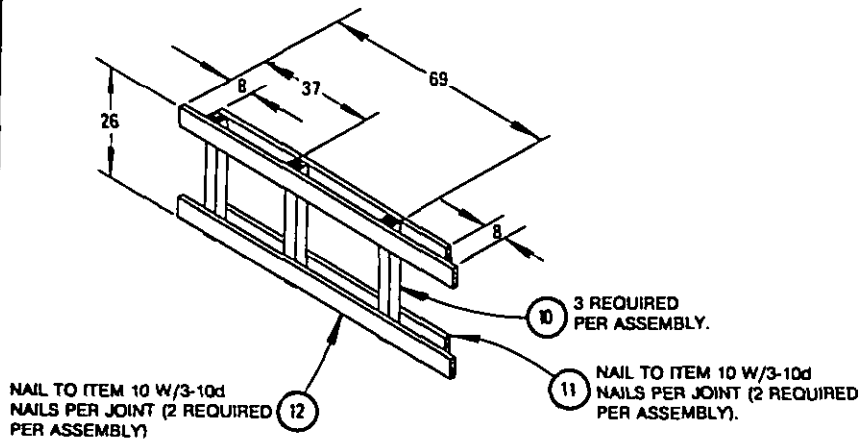
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DETAIL B



DETAIL C



DETAIL D

* 'TW-1' MEANS INSIDE TRAILER WIDTH MINUS ONE INCH.

| ITEM | DESCRIPTION | MAT'L/DWG | DIMENSIONS |
|------|--------------------|----------------|---------------|
| 12 | AFT SPACER | WOOD, DETAIL D | 2 X 6 X TW-1* |
| 11 | FWD SPACER | WOOD, DETAIL D | 2 X 6 TO SUIT |
| 10 | VERTICAL | WOOD, DETAIL D | 4 X 4 X 28 |
| 9 | REAR FILLER SPACER | WOOD, DETAIL C | 2 X 4 TO SUIT |
| 8 | REAR FILLER | WOOD, DETAIL C | 2 X 6 X TW-1* |
| 7 | STRUT | WOOD, DETAIL B | 2 X 6 TO SUIT |
| 6 | VERTICAL SPACER | WOOD, DETAIL B | 2 X 4 TO SUIT |
| 5 | HORIZONTAL SPACER | WOOD, DETAIL B | 2 X 6 TW-1* |
| 4 | REAR FACE BOARD | WOOD, DETAIL B | 2 X 6 X 69 |

ALTERNATE FTL 40 THRU 48 FT TRAILERS

WHEN THIS DISTANCE IS LESS THAN 75 INCHES, USE THE BLOCKING PROCEDURE SHOWN IN DETAIL E TO SEPARATE THE FORWARD AND AFT PORTIONS OF THE TRUCKLOAD.

