

MILITARY STANDARD

MIL-STD-1320-253A
(NAVY)

TRUCKLOADING

UNIT LOADS OF

AMRAAM, AIM-120A IN S/S/C, CNU-415A/E
ORAMRAAM JAIM-120A CAPTIVE
AIR TRAINING MISSILE (CATM)
IN S/S/C CNU-555/E

26 OCT 1993

SUPERSEDING
MIL-STD-1320-253
8 MAY 1991

CONTAINER DATA

NUMBER OF MISSILES/CONTAINER _ _ _ _ _ 4

GROSS WEIGHT OF ONE CONTAINER (EMPTY) _ _ _ _ _ 795 LBS.

GROSS WEIGHT OF ONE CONTAINER (LOADED) _ _ _ _ _ 2,075 LBS.

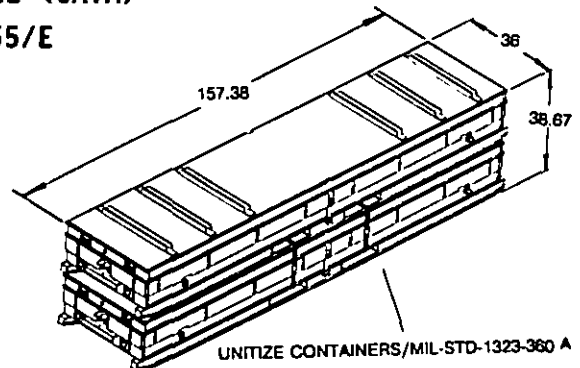
GROSS WEIGHT OF UNIT LOAD _ _ _ _ _ 4,158 LBS. Δ

CUBE _ _ _ _ _ 128.8 CU. FT.

 Δ DO NOT USE FOR SHIPPING WEIGHT

FOR HAZARD CLASSIFICATION SEE SW020-AC-SAF-010/020/030

DATA SHOWN IS FOR BOTH CONTAINERS.



AMSC NO. - N/A

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NOTES:

1. UNLESS OTHERWISE SPECIFIED, DIMENSIONS ARE IN INCHES.
2. FOR CROSS REFERENCE TO ASSOCIATED PALLETIZING, CONTAINERLOADING AND CARLOADING MILITARY STANDARDS, REFER TO INDEX TO STANDARDS, MIL-HDBK-236 (NAVY).

FSC 8140

A	ADDED S/S/C CNU-555/E & UP DATED DOCUMENT	5/8/91	DR	ALC
REV LTR	REVISION DESCRIPTION	DATE	TDA	SYSCOM
			APPROVAL	

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E. Linder 5/8/91
SIGNATURE TDA, WPNSTA EARLE DATE

William O. Schuster 5/9/91
SIGNATURE AIRSYSOM BY DIRECTION DATE

ORIGINATOR WPNSTA EARLE 5/8/91
DATE

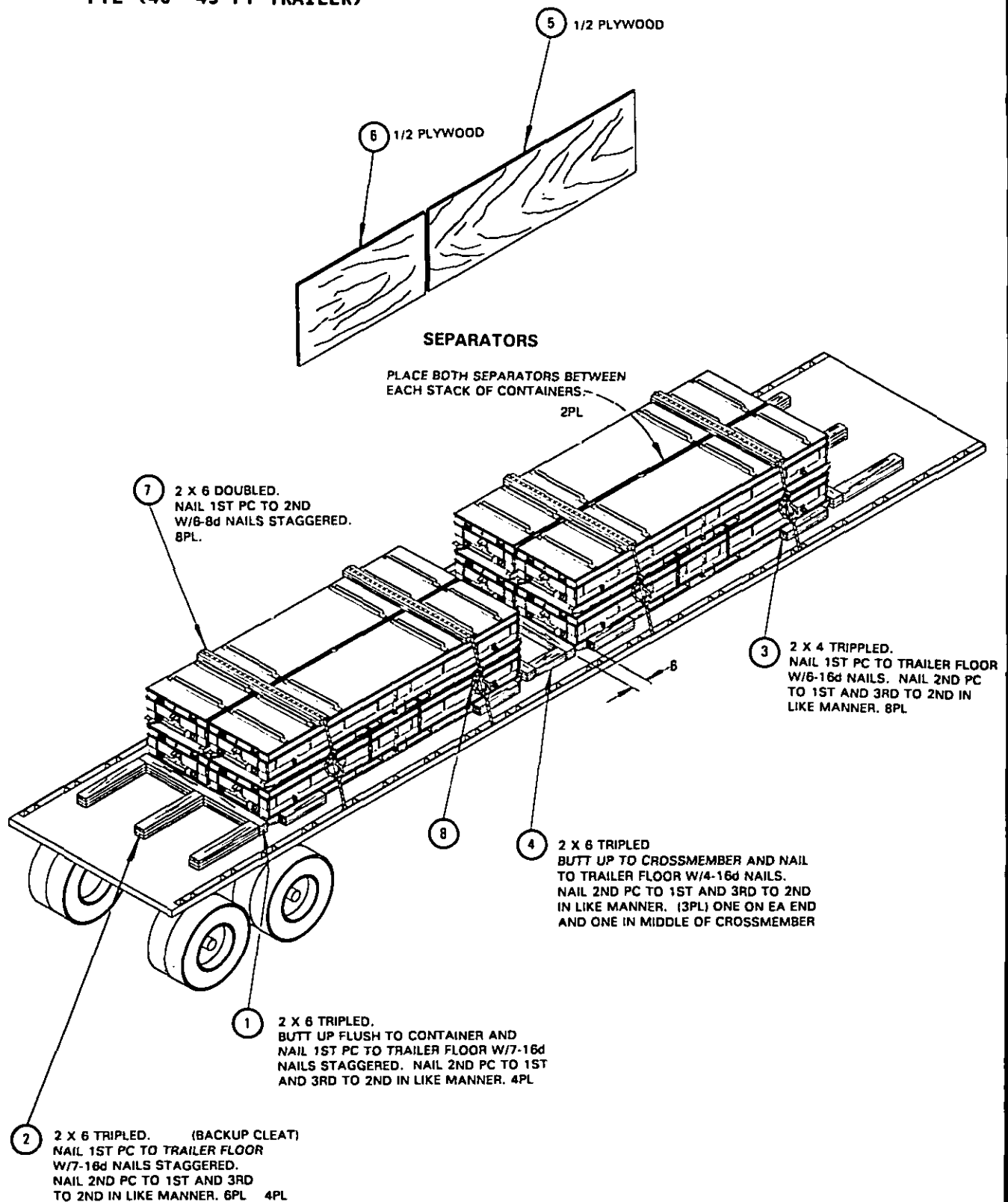
WPNSTA EARLE, NEW JERSEY

PAGE 1 OF 5

MIL-STD-1320-253A (NAVY)**GENERAL NOTES:**

1. THIS DOCUMENT GIVES DETAILED INSTRUCTIONS FOR TRUCKLOADING THE AMRAAM AIM-120A IN S/S/C, CNU-415A/E OR AMRAAM JAIM-120A IN S/S/C CNU-555/E
2. THE PROCEDURES DESCRIBED HEREIN ARE INTENDED FOR 40 FT OR LONGER FLATBED TRAILERS. DO NOT USE TRAILERS WITH ALL METAL FLOORS.
3. CHAINS AND STEEL STRAPPING MAY BE USED INTERCHANGEABLY ON A ONE-TO-ONE BASIS FOR TIEDOWNS.
4. STEEL STRAPPING USED AS TIEDOWNS SHALL BE 2 X .050 AND SHALL CONFORM TO AND BE APPLIED AS SPECIFIED IN THE GENERAL TRUCKLOADING DOCUMENT, MIL-STD-1320 (NAVY)
5. CHAINS, FITTINGS, AND LOAD BINDER SHALL CONFORM TO AND BE APPLIED AS SPECIFIED IN THE GENERAL TRUCKLOADING DOCUMENT, MIL-STD-1320 (NAVY) EXCEPT THAT THE CHAIN/GRAB HOOKS SHALL BE ATTACHED TO THE STAKE POCKETS (NOT AROUND THE RUB RAIL).
6. PROTECTOR BOARDS, ITEM 7, SEE PAGE 3, SHALL BE USED UNDER THE TIEDOWNS. INSTALL PROTECTOR BOARD ASSY OVER EACH OF THE OUTBOARD STACKING CHANNELS. WHEN USING STEEL STRAPPING, THE STRAPS SHALL BE STAPLED TO THE PROTECTOR BOARD USING 2" STRAPPING STAPLES. WHEN USING CHAIN, THE CHAINS SHALL BE SECURED TO THE PROTECTOR BOARD BY DRIVING A 10d NAIL THROUGH THE CHAIN LINK AND CLINCHING THE NAIL OVER THE CHAIN. FIVE STAPLES/NAIL SHALL BE USED FOR EACH TIEDOWN.
7. THE MAXIMUM NUMBER OF CONTAINERS THAT CAN BE LOADED ONTO A TRAILER IS:
40 THRU 45 FT TRAILERS - 8 CONTAINERS (4 UNIT LOADS).
48 FT TRAILER - 12 CONTAINERS (6 UNIT LOADS).
8. PRIOR TO LOADING THE TRAILER AND DURING PRELOADING INSPECTION REQUIRED BY NAVSEA OP 2165 AND REPORTED ON DD FORM 636, THE CHAINS, FITTINGS, AND LOAD BINDERS SHALL BE INSPECTED FOR STRETCH, GOUGING, BENT LINKS, WEAR, AND ANY OTHER NOTICEABLE DEFECTS. THE INSPECTOR SHALL CONFIRM THAT THE CHAINS, FITTINGS, AND LOAD BINDERS HAVE BEEN INSPECTED AND SHALL SO NOTE IN ITEM NO. 22 OF DD FORM 626. ANY DEFICIENCY SHALL BE CAUSE FOR REJECTION OF A CHAIN OR LOAD BINDER.
9. UNLESS OTHERWISE SPECIFIED, ALL DUNNAGE LUMBER, STEEL STRAPPING, STRAP SEALS, AND NAILS SHALL BE AS SPECIFIED IN THE GENERAL DOCUMENT MIL-STD-1320 (NAVY).
10. THE MAXIMUM GROSS WEIGHT OF THE TRACTOR-TRAILER AND THE ALLOWABLE AXLE WEIGHTS ARE THE RESPONSIBILITY OF THE CARRIER. THE CARRIER WILL ADVISE THE SHIPPER OF THESE LIMITATIONS, AND THE SHIPPER SHALL LOAD THE TRAILER IN SUCH A MANNER THAT THE TRACTOR-TRAILER WILL NOT EXCEED THESE LIMITATIONS.
11. AFTER THE BLOCKING AND BRACING HAS BEEN INSPECTED, ATTACH THE SHIPPING DOCUMENTS TO AN ACCESSIBLE AREA INSIDE THE TRAILER. ATTACH THE PROPER EXPLOSIVES PLACARDS TO BOTH SIDES, FRONT, AND BACK OF THE TRAILER.
12. FOR GENERAL TRUCKLOADING PROCEDURES REFER TO THE GENERAL TRUCKLOADING DOCUMENT, MIL-STD-1320 (NAVY).

FTL (40 - 45 FT TRAILER)

**NOTE:**

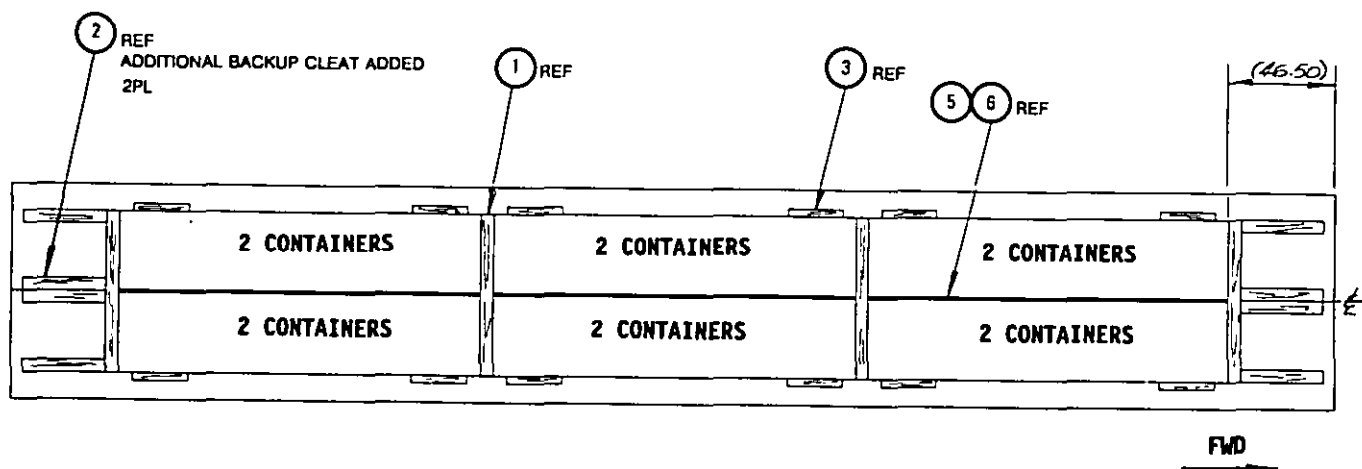
A 40 FT TRAILER WITH A MAX. OF (8) CONTAINERS IS SHOW HERE FOR DEMONSTRATION PURPOSE ONLY. IF A 48 FT TRAILER IS AVAILABLE TWO MORE UNITIZED STACKS (4) CONTAINERS CAN BE ADDED FOR A TOTAL OF 12 CONTAINERS/TRUCKLOAD. (SEE PAGE 4) FOR BLOCKING & BRACING OF 12 CONTAINERS.

MIL-STD-1320-253A (NAVY)

FTL 48 FT (FLATBED TRAILER)

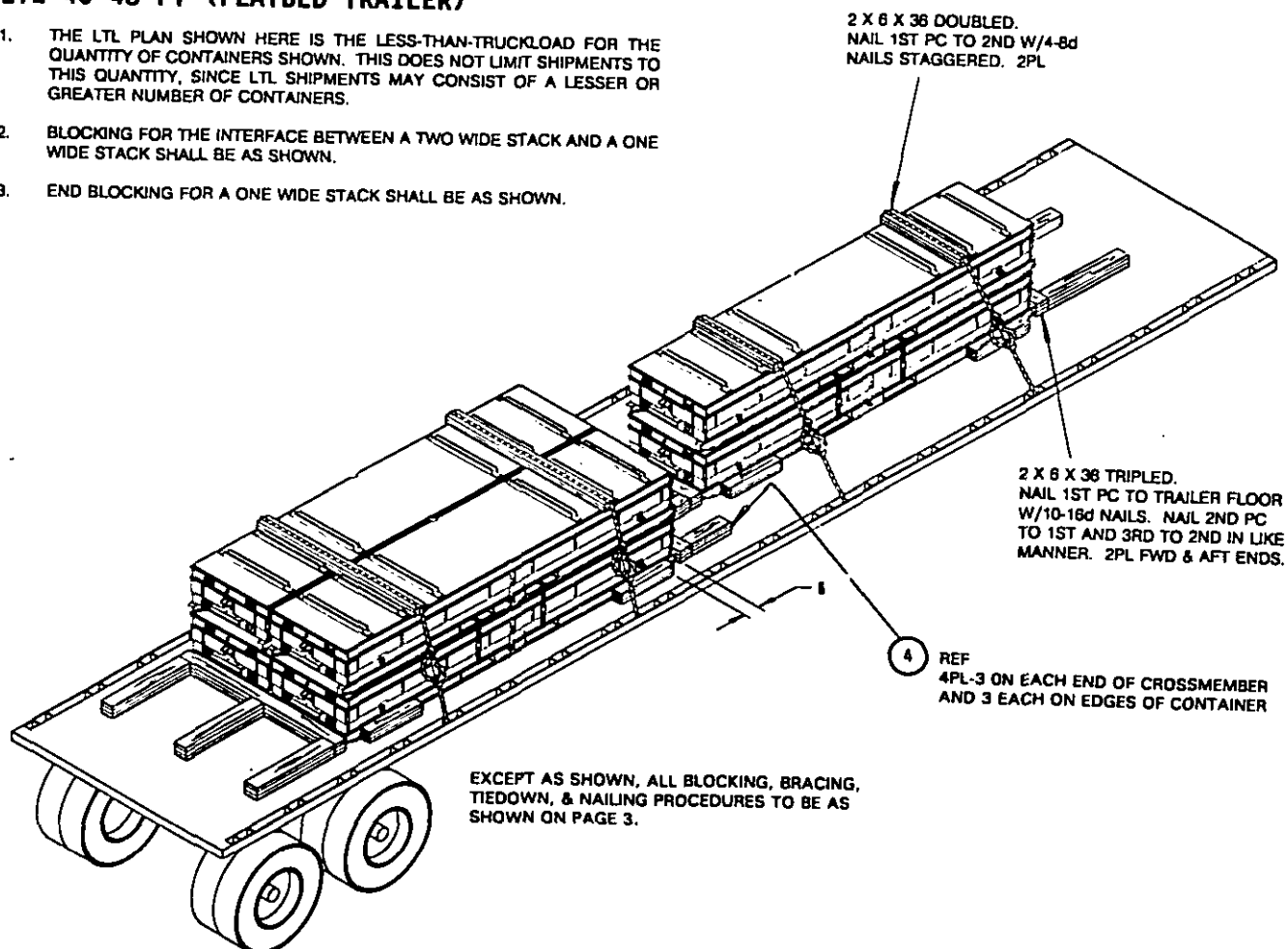
NOTE:

NOTE WHEN A 48 FT TRAILER IS AVAILABLE ADD AN ADDITIONAL TWO STACKS (4) CONTAINERS AND BLOCK AND BRACE AS SHOWN. NAILING PROCEDURES AND TIEDOWN METHODS TO REMAIN THE SAME AS SHOWN ON PAGE 3 FOR (B) CONTAINERS.



LTL 40-48 FT (FLATBED TRAILER)

1. THE LTL PLAN SHOWN HERE IS THE LESS-THAN-TRUCKLOAD FOR THE QUANTITY OF CONTAINERS SHOWN. THIS DOES NOT LIMIT SHIPMENTS TO THIS QUANTITY, SINCE LTL SHIPMENTS MAY CONSIST OF A LESSER OR GREATER NUMBER OF CONTAINERS.
2. BLOCKING FOR THE INTERFACE BETWEEN A TWO WIDE STACK AND A ONE WIDE STACK SHALL BE AS SHOWN.
3. END BLOCKING FOR A ONE WIDE STACK SHALL BE AS SHOWN.



4	8	CHAIN & BINDER	STEEL	3/8
8	7	PROTECTOR BOARD	WOOD	2 X 6 X 72 1/2
2	6	SEPARATOR (SHORT)	PLYWOOD	1/2 X 34 X 60
2	5	SEPARATOR (LONG)	PLYWOOD	1/2 X 34 X 96
9	4	BACKUP CLEAT (MIDDLE)	WOOD	2 X 6 X 18
24	3	SLEEPER		2 X 4 X 24
18	2	BACKUP CLEAT		2 X 6 X 36
12	1	CROSSMEMBER	WOOD	2 X 6 X 72
REQ	ITEM	DESCRIPTION	MAT'L/DWG	DIMENSIONS
LIST OF MATERIALS				