

INCH-POUND
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 SUPERSEDING
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MILITARY SPECIFICATION

BAR, REPEATABLE RELEASE HOLDBACK, AIRCRAFT LAUNCHING, GENERAL DESIGN REQUIREMENTS FOR

This specification is approved for use by the Naval Air Systems Command, Department of the Navy, and is available for use by all Departments and Agencies of the Department of Defense.

1. SCOPE

1.1 Scope. This specification establishes the requirements for the design, construction, performance, and test of the repeatable release holdback bar (RRHB) for use on carrier-type aircraft.

2. APPLICABLE DOCUMENTS

2.1 Government documents.

2.1.1 Specifications, standards, and handbooks. The following specifications, standards, and handbooks form a part of this document to the extent specified herein. Unless otherwise specified, the issues of these documents are those listed in the issue of the Department of Defense Index of Specifications and Standards (DODISS) and supplement thereto, cited in the solicitation (see 6.2).

SPECIFICATIONS

FEDERAL

QQ-C-320	Chromium Plating (Electrodesposited)
QQ-N-290	Nickel Plating (Electrodeposited)
QQ-P-416	Plating, Cadmium (Electrodeposited)
QQ-S-365	Silver Plating Electrodeposited: General Requirements for

Beneficial comments (recommendations, additions, deletions) and any pertinent data which may be of use in improving this document should be addressed to: Systems Engineering and Standardization Department (Code 53), Naval Air Engineering Center, Lakehurst, N.J. 08733-5100, by using the Standardization Document Improvement Proposal (DD Form 1426) appearing at the end of this document or by letter.

AMSC N/A

FSC 1720

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MIL-B-85110A(AS)

SPECIFICATIONS (Continued)

MILITARY

DOD-D-1000	Drawings, Engineering and Associated Lists
MIL-C-5541	Chemical Conversion Coatings on Aluminum and Aluminum Alloys
MIL-H-6088	Heat Treatment of Aluminum Alloys
MIL-H-6875	Heat Treatment of Steel, Process for
MIL-F-7179	Finishes, Coatings, and Sealants for the Protection of Aerospace Weapons Systems
MIL-F-7190	Forging, Steel, for Aircraft/Aerospace Equipment and Special Ordnance Applications
MIL-S-8516	Sealing Compound, Polysulfide Rubber, Electric Connectors and Electric Systems, Chemically Cured
MIL-A-8625	Anodic Coatings, for Aluminum and Aluminum Alloys
MIL-C-8837	Coating, Cadmium (Vacuum Deposited)
MIL-A-8863	Airplane Strength and Rigidity Ground Loads for Navy Acquired Airplanes
MIL-T-10727	Tin Plating: Electrodeposited or Hot-Dipped, for Ferrous and Nonferrous Metals
MIL-P-15024	Plates, Tags and Bands for Identification of Equipment
MIL-F-18264	Finishes: Organic, Weapons System, Application, and Control of
MIL-A-21180	Aluminum Alloy Castings, High Strength
MIL-A-22771	Aluminum Alloy Forgings, Heat Treated
MIL-D-23003	Deck Covering Compound, Nonslip, Rollable
MIL-S-23586	Sealing Compound, Electrical, Silicone Rubber, Accelerator Required
MIL-M-24041	Molding and Potting Compound, Chemically Cured, Polyurethane
MIL-C-26074	Coatings, Electroless Nickel, Requirements for

MIL-B-85110A(AS)

SPECIFICATIONS (Continued)

MILITARY

MIL-M-43719	Marking Materials and Markers, Adhesive Elastomeric, Pigmented, General Specification for
MIL-H-46855	Human Engineering Requirements for Military Systems, Equipment and Facilities
MIL-H-81200	Heat Treatment of Titanium and Titanium Alloys

STANDARDS

FEDERAL

FED-STD-595	Colors
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MILITARY

DOD-STD-100	Engineering Drawing Practices
MIL-STD-130	Identification Marking of U.S. Military Property
MIL-STD-810	Environmental Test Methods and Engineering Guidelines
MIL-STD-838	Lubrication of Military Equipment
MIL-STD-889	Dissimilar Metals
MIL-STD-970	Standards and Specifications, Order of Preference for the Selection of
MIL-STD-1472	Human Engineering Design Criteria for Military Systems, Equipment and Facilities
MIL-STD-1949	Inspection, Magnetic Particle
MIL-STD-2175	Castings, Classification and Inspection of

HANDBOOKS

MIL-HDBK-5 Vol 1 and Vol 2	Metallic Materials and Elements of Aerospace Vehicle Structures
MIL-HDBK-132	Protective Finishes for Metal and Wood Surfaces
MIL-HDBK-694	Aluminum and Aluminum Alloys

BULLETINS

MIL-BUL-147	Specifications and Standards for Non-Government Organizations Released for Flight Vehicle Design and Construction
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MIL-B-85110A(AS)

(Unless otherwise indicated, copies of federal and military specifications, standards, handbooks, and bulletins are available from the Standardization Documents Order Desk, Building 4D, 700 Robbins Avenue, Philadelphia, PA 19111-5094.)

2.1.2 Other Government documents, drawings, and publications. The following other Government documents, drawings, and publications form a part of this specification to the extent specified herein. Unless otherwise specified, the issues are those cited in the solicitation.

DRAWINGS

NAVAL AIR ENGINEERING CENTER (CAGE Code 80020)

607770	Design Requirements, Catapulting Arrangement, Nose Gear Type Launch
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(Copies are available from the Naval Air Technical Services Facility (NATSF) (Code 312), 700 Robbins Avenue, Philadelphia, PA 19111-5097. Copies are also available from the Naval Air Engineering Center, Ship and Shore Installation Engineering Dept (Code 5144) Lakehurst, NJ 08733-5000.)

PUBLICATIONS

NAVAL AIR SYSTEMS COMMAND

SD-24	General Specification for Design and Construction of Aircraft Weapons Systems. Fixed Wing Aircraft
Volume 1	

AD-1350	Engineering Drawings and Associated Data
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(Copies are available from the Naval Air Systems Command, Standardization Section (Code 51122E), Washington, DC 20361-5110.)

INSTRUCTIONS

OFFICE, CHIEF OF NAVAL OPERATIONS

OPNAVINST 4790.2	The Naval Aviation Maintenance Program (NAMP)
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NAVPERS 18068	Manual of Qualifications for Advancement in Rating
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(Copies of OPNAVINST are available from the Chief of Naval Operations, Code 09B39H, Pentagon Bldg, Washington, DC 20350. Copies of NAVPERS are available from the Aviation Supply Office (ASO), Code 10151, 5801 Tabor Avenue, Philadelphia, PA 19120-5099.)

2.2 Non-Government publications. The following document(s) form a part of this document to the extent specified herein. Unless otherwise specified, the issues of the documents which are DoD adopted are those listed in the issue of the DODISS cited in the solicitation. Unless otherwise specified, the issues of documents not listed in the DODISS are the issues of the documents cited in the solicitation (see 6.2).

MIL-B-85110A(AS)

AMERICAN SOCIETY FOR TESTING AND MATERIALS (ASTM)

ASTM B633

Standard Specification for Electrodeposited
Coatings of Zinc on Iron and Steel (DoD adopted)

(Application for copies should be addressed to the American Society for Testing and Materials, 1916 Race Street, Philadelphia, PA 19103-1187.)

(Non-Government standards and other publications are normally available from the organizations that prepare or distribute the documents. These documents also may be available in or through libraries or other informational services.)

2.3 Order of precedence. In the event of a conflict between the text of this document and the references cited herein, the text of this document takes precedence. Nothing in this document, however, supersedes applicable laws and regulations unless a specific exemption has been obtained.

3. REQUIREMENTS

3.1 Selection of materials and standard parts. The selection of materials, standard parts, processes and corrosion protection shall be in accordance with the requirements of SD-24, Volume 1.

3.1.1 Materials. Materials shall conform to applicable specifications and shall be as specified herein and on applicable drawings. Design shall make maximum use of standard (MS, AN, MIL-STD) parts, materials and processes, rather than special or peculiar items. Materials which are not covered by government specifications, or which are not specifically described herein, shall be of the best quality, suitable for the purpose intended, and shall be approved by the acquiring activity. Particular care shall be given to close fitting parts in the choice of both materials and corrosion prevention method.

3.1.1.1 Metal parts. All metal parts shall be of the corrosion-resistant type or treated in a manner to render them resistant to corrosion. Unless suitably protected against electrolytic corrosion, dissimilar metals, as defined in MIL-STD-889, shall not be used in contact with each other. General design information governing usage of metals is furnished in MIL-HDBK-5, Volumes 1 and 2. General design information for aluminum and aluminum alloys is provided in MIL-HDBK-694. Metals which are not covered by government specifications or which are not specifically described herein, shall be of the best quality, suitable for the purpose intended, and shall be approved by the acquiring activity.

3.1.1.1.1 Heat treatment. Heat treatment of aluminum parts and steel parts shall be in accordance with MIL-H-6088 and MIL-H-6875, respectively. Heat treatment of titanium and titanium alloy parts shall be in accordance with MIL-H-81200.

3.1.1.1.2 Magnetic particle inspection. Magnetic particle inspection of all ferromagnetic materials shall be in accordance with MIL-STD-1949.

3.1.1.1.3 Castings. Castings shall conform with the requirements of MIL-STD-2175. In addition, aluminum alloy castings shall conform to the requirements of MIL-A-21180.

MIL-B-85110A(AS)

3.1.1.1.4 Forgings. Steel forgings shall conform to the requirements of MIL-F-7190. Critical steel forgings shall meet the requirements for MIL-F-7190 Grade A forgings. Aluminum forgings shall conform to the requirements of MIL-A-22771.

3.1.1.1.5 Magnesium and magnesium alloys. Magnesium and magnesium alloy parts shall not be used.

3.1.1.2 Non-metallic components. Non-metallic components shall be designed to minimize deterioration caused by abrasion and exposure to sunlight, microorganism, fungi, moisture, heat, fuel, hydraulic and lubricating oil and grease, and salt spray. Protection shall be provided for those non-metallic components for which strength degradation associated with abrasion or exposure-induced deterioration can endanger the system user. All cover materials shall be flame resistant and shall not produce toxic gases when subjected to high temperature or flame. Materials that are nutrients for fungi shall be treated with a fungicidal agent approved by the Government acquiring activity. Under no circumstances shall polyester elastomers be used in fabricating molded components or potting or sealing applications without specific approval. Flexible epoxy resins and adhesives and other flexible materials having ester linkages shall be tested for hydrolytic stability.

3.1.1.3 Lubrication. Lubricants and lubrication practices shall conform to the requirements of MIL-STD-838. Any lubricants not meeting the requirements of MIL-STD-838 shall require approval from the acquiring activity for their use. Lubricants shall function satisfactorily throughout the temperature range as specified in 3.4.5.1. Choice of lubricants shall: (a) reduce the hazards to non-metallic system components, (b) reduce damage to finishes adjacent to location of lubricant application, and (c) eliminate the need for frequent relubrication by field maintenance activities. If relubrication is required, choice of lubricants and practices should be such that relubrication need be accomplished only during inspection, calibration or overhaul periods.

3.1.1.4 Decalcomanias. Decalcomanias shall conform to the requirements of MIL-STD-43719.

3.1.1.5 Potting compounds. Potting compounds shall be selected from those listed on the Qualified Products List (QPL) for MIL-S-8516, MIL-S-23586, and MIL-M-24041 and which have completed tests to the Government acquiring activity's satisfaction to demonstrate their hydrolytic stability.

3.1.1.6 Fungus-proof materials. To the greatest extent practicable, the materials used in the RRHB shall be non-nutrients for fungi. If materials that are nutrients for fungi must be utilized, such materials shall be approved by the acquiring activity.

3.1.2 Corrosion protection. Protective coatings and finishes shall be in accordance with MIL-F-7179 unless otherwise specified in the RRHB detail specification.

3.1.2.1 Finishes. General guidance in the application and control of organic finishes is provided in MIL-F-18264 and MIL-HDBK-132.

MIL-B-85110A(AS)

3.1.2.2 Anodizing. All aluminum and aluminum alloy parts, except those subject to wear, shall be anodized in accordance with MIL-A-8625, Type II anodic coating. Anodic coatings for all aluminum and aluminum alloy parts subject to wear shall conform to MIL-A-8625, Type III. Fatigue critical aluminum and aluminum alloy parts shall utilize MIL-A-8625, Type I anodic coating or aluminum (99.5% pure) coating.

3.1.2.2.1 Chemical surface treatment. For aluminum and aluminum alloy parts not subject to wear, abrasion or erosion, chemical conversion surface treatment in accordance with MIL-C-5541 may be used in lieu of anodizing if the parts are to be painted.

3.1.2.3 Metal coatings. The metals used in the RRHB shall possess adequate corrosion resistant characteristics or shall be suitably protected by the use of coatings equivalent to those listed in Table I to resist corrosion which may result from such conditions as, but not limited to, dissimilar metal combinations, moisture, salt spray, and high temperature deterioration. Where not indicated, class or type is at the option of the manufacturer, subject to approval by the acquiring activity. Dissimilar metals are defined in MIL-STD-889. Unless otherwise specified herein, physical properties of all metals shall meet the minimum requirements of MIL-HDBK-5, Volumes 1 and 2.

TABLE I. Metal coatings.

Coating	Specification
Cadmium plating	QQ-P-416, type II, class 2 <u>1/</u>
Cadmium plating	MIL-C-8837, type II class 1 <u>2/</u>
Zinc plating	ASTM B633, type II, class 2
Chromium plating	QQ-C-320
Nickel plating	QQ-N-290
Silver plating	QQ-S-365
Tin plating	MIL-T-10727, type I
Electroless nickel	MIL-C-26074

1/ For steels heat treated up to 180,000 psi.

2/ For steels heat treated above 180,000 psi.

3.2 Selection of specifications and standards. Specifications and standards for necessary commodities and services not specified herein shall be selected in accordance with MIL-STD-970. A partial listing of approved (MIL-STD-970 Group II) non-government organization specifications and standards is furnished in MIL-BUL-147.

MIL-B-85110A(AS)

3.3 Drawings. Drawing requirements shall be specified by the acquiring activity in accordance with DOD-STD-100 and AD-1350 instructions. Generally all categories of drawings established in DOD-D-1000 will be required. Unless otherwise directed by the Government acquiring activity, all drawings shall conform to the requirements established in DOD-D-1000 for Level 2 Drawings.

3.4 Design and construction. The RRHB shall be designed and constructed to meet the requirements of this specification and the applicable drawings.

3.4.1 Dimensions. The RRHB shall be designed with the deck end configurations and dimensions as shown in NAEC Drawing 607770.

3.4.2 Weight. The weight of the RRHB shall be kept to a minimum. The contractor shall conduct a weight control program during the design phase to ensure a design of minimum weight.

3.4.3 Operating characteristics. The RRHB shall meet the following functional requirements.

- (a) The RRHB shall not depend upon failure type elements for operation.
- (b) The RRHB shall permit rapid and easy attachment to the aircraft, and be firmly held after installation.
- (c) The RRHB shall be designed to operate satisfactorily when it is rotated 180° about its longitudinal axis during installation.
- (d) The installed RRHB shall not be free to rotate about its longitudinal axis and thus prevent proper engagement with the buffer slider.
- (e) The RRHB shall carry any load up to its required release load without releasing prematurely.
- (f) The RRHB shall release repeatedly within the load ranges specified in MIL-A-8863 throughout the entire operating conditions specified in 3.4.4 and 3.4.5.
- (g) The RRHB shall operate satisfactorily and be insensitive to catapult loading rates up to 800,000 pounds/second.
- (h) The installed RRHB shall be capable of being tracked into the buffer of the nose gear launch deck hardware at a maximum speed, which shall be the smaller of:
 - (1) Four knots, or
 - (2) That speed which will cause a maximum load in the holdback bar, during buffing,

MIL-B-85110A(AS)

equal to the minimum release load as specified in MIL-A-8863.

- (i) The RRHB shall be incapable of being installed on an aircraft other than that of which it was designed.
- (j) The RRHB shall be capable of absorbing internally any strain energy stored in the unit through a self-contained snubber.
- (k) The RRHB shall incorporate a mechanical override feature to permit manual release up to load P. The manual release load, P in pounds, in the RRHB is:

$$P = \frac{T + .2W}{\cos \theta}$$

where:

T = idle thrust on 59°F day

W = airplane maximum gross weight

θ = angle between holdback axis and the deck

- (l) The RRHB shall automatically reset itself for subsequent reuse after final release from the aircraft holdback fitting within twenty seconds.
- (m) The RRHB shall be free of any external adjustments that could permit inadvertent or unauthorized variation in release load. An adjustment feature shall be provided which will permit periodic calibration of the release load as required.
- (n) The RRHB shall incorporate a counter to record automatically the number of cycles of operation for the purpose of determining remaining service life.
- (o) The RRHB shall be capable of being installed on or removed from an aircraft by one crewman.
- (p) An overload control section shall be provided either on the aircraft holdback fitting or on the aircraft end of the RRHB, as dictated by the aircraft design and approved by the acquiring activity. The design of the overload control section shall be such that failure at this point shall not impair normal functions of the nose landing gear, or otherwise cause damage to the aircraft.

MIL-B-85110A(AS)

- (q) The RRHB shall provide a visual indication of having reset itself.
- (r) The RRHB shall provide a visual indication of correct installation to the airplane catapult holdback fitting.
- (s) The RRHB shall be designed so that any internal failure will cause the indicator device(s) to show an unsafe condition,, and prevent subsequent application of initial tension loading.

3.4.4 Loads. Strength is required for the catapulting, ground handling, release, and a fatigue loads as specified in 3.4.4.1 through 3.4.4.4.

3.4.4.1 Ultimate. The RRHB shall have ultimate load limits that will not damage or change the operating characteristics when the ultimate load test conditions of (a) and (b) are performed (see 4.3.3.1). Ultimate load test conditions are:

- (a) Maximum tensile load of the RRHB shall be (1.50) times (1.06) times the "R" value as defined in MIL-A-8863.
- (b) RRHB rigidly supported at both ends and a 400 pound static load applied in any direction at the midpoint normal to the longitudinal axis.

3.4.4.2 Endurance. The components must withstand the entire fatigue spectrum of Figure 1 for catapulting loads, including a scatter factor of 2.0 both by analysis and the endurance tests of 4.3.3.5.

3.4.4.3 Release. Release loads shall be those specified in MIL-A-8863.

3.4.4.4 Overload. The overload control section shall be designed in such a manner that the aircraft will never be loaded greater than 1.60R (see 3.4.3(r) and 3.4.4.1 (a)) due to the bar malfunctioning.

3.4.5 Environmental. The RRHB shall operate satisfactorily with no malfunctions under any of the following environmental conditions (see 3.4.5.1 through 3.4.5.7) or combinations of these conditions as specified herein.

3.4.5.1 Temperature. The RRHB shall operate satisfactorily with no malfunctions when subjected to the ambient temperature ranges and conditions of (a) and (b) and the high and low temperature tests of 4.3.2.2 and 4.3.2.3.

- (a) Storage -70°F to +160°F for extended period of time.
- (b) Operating Air, -40°F to +160°F at sea level.

3.4.5.2 Low pressure. The RRHB shall operate satisfactorily with no malfunctions when subjected to the low pressure atmosphere and the low pressure atmosphere and the low pressure tests of 4.3.2.1.

MIL-B-85110A(AS)

3.4.5.3 Dust. The RRHB shall operate satisfactorily with no malfunctions when:

- (a) Exposed to the dust and grit particles in combination with hydraulic fluid, jet exhaust residue and other oily films common to an aircraft carrier deck during flight operations.
- (b) Subjected to the dust tests as specified in 4.3.2.4.

3.4.5.4 Humidity. The RRHB shall operate satisfactorily with no malfunctions when:

- (a) Exposed to humidities up to 100%, including conditions where condensation, including frost, takes place in or on the RRHB.
- (b) Subjected to the humidity tests of 4.3.2.5.

3.4.5.5 Salt fog. The RRHB shall operate satisfactorily with no malfunctions when exposed to the operational salt sea atmosphere and the salt fog test of 4.3.2.6.

3.4.5.6 Vibration. The RRHB shall be designed to meet the performance requirements of this specification when subjected to the vibration tests of 4.3.2.7.

3.4.5.7 Rough usage. The RRHB shall be designated to withstand the type of usage that will be encountered on an aircraft carrier, including being thrown and dragged on carborundum coated steel decks, and stored or used when subjected to wet conditions (including rain, salt water, steam, salt air environment, oil, greases).

3.4.6 Reliability. Equipment reliability shall be considered in every phase of the design process. Techniques used to determine and evaluate the detailed design include analyses, part selection, derating, and appropriate application of design guidelines. The acquiring activity shall specify the mean cycles between failures (MCBF) for each procurement. In meeting this requirement, the scheduled preventive maintenance recommended by the contractor for service usage and acceptable to the acquiring activity will be permitted. Successful completion of the testing herein shall not relieve the contractor of the responsibility for compliance of production units with the specified reliability and performance requirements during any subsequent testing or service usage within the limitations specified.

3.4.7 Maintainability. The RRHB shall be designed in accordance with the quantitative and qualitative maintainability requirements as specified in 3.4.7.1 and 3.4.7.2. The RRHB shall be maintained at the intermediate level of maintenance as specified in OPNAVINST 4790.2 and 3.4.7.1.3.

3.4.7.1 Quantitative.

3.4.7.1.1 Minimum cycles between overhaul. The equipment shall be designed such that it is capable of a minimum of (to be specified by the Naval Air Systems Command (NAVAIR)) cycles between overhauls. A cycle is defined by

MIL-B-85110A(AS)

Figure 1. Replacement of critical parts subject to wear/deformation is permitted after each (to be specified by NAVAIR) cycles.

3.4.7.1.2 Organizational level. Organizational level maintenance generally shall be unnecessary.

3.4.7.1.2.1 Scheduled. There shall be no scheduled maintenance other than a calibration check every (to be specified by NAVAIR) cycles.

3.4.7.1.3 Intermediate level. All intermediate level repairs of the RRHB shall not exceed a mean time of 2.0 manhours. The manhours shall include, but not be limited to, the following:

- (a) Connect and disconnect any test equipment.
- (b) Bench check
- (c) Disassembly and assembly
- (d) Adjustments
- (e) Calibrations if required
- (f) Functional checkout for repair verification

90% of all repair actions shall be completed within 2.83 manhours. The mean time to completely disassemble and reassemble the RRHB shall not exceed 3.0 manhours.

3.4.7.1.4 Depot level. There shall be no depot level maintenance.

3.4.7.1.5 Support equipment. Requirements for additional equipment shall be held to a minimum. Requirements for peculiar ground support equipment (PGSE) and special tools shall be justified, documented and submitted to the acquiring activity.

3.4.7.2 Qualitative.

3.4.7.2.1 Preventive maintenance. The RRHB shall be designed to eliminate requirements for scheduled maintenance. In the event that this requirement cannot be achieved, proposed requirements shall be justified by the contractor and will be subject to NAVAIR approval. Scheduled replacement will not be allowed for any parts unless the contractor has established that such parts have a wearout or fatigue characteristics which results in a determinable life span with nonrandom life distribution. Wearout of parts caused by mechanical operation is a typical example which may justify a scheduled maintenance action. Where parts replacement is considered necessary to preclude failure, service or wear tolerances for affected parts shall be included on the contractor's drawings.

3.4.7.3 Personnel skill level. Organizational and intermediate level of maintenance personnel shall not exceed a skill level E-6 as defined in NAVPERS 18068, Occupational field 5 for at least 95% of the maintenance tasks. A man of lower skill may assist in disassembly or assembly.

3.4.8 Storage life. The RRHB shall have a storage life of not less than 36 months when stored in an unheated warehouse using Level A packing condition as specified in the contract. Storage life is the period for which

MIL-B-85110A(AS)

the equipment may be kept in storage before installation and still have the specified operational service life after installation without component or part replacement, adjustment, or maintenance action.

3.4.9 Human engineering. In addition to complying with the human engineering requirements of MIL-STD-1472 and MIL-H-46855, the RRHB design shall include, but not be limited to, the following:

- (a) The RRHB shall provide for installation and removal by no more than one crewman.
- (b) No degradation of crewman performance shall be allowed while installing and removing the RRHB due to personnel wearing essential clothing required of the operating environment.
- (c) The RRHB shall incorporate physical measures to preclude interchange of units or components of same or similar form that are not in fact functionally interchangeable.
- (d) The RRHB shall incorporate physical measures to preclude improper mounting of units or components.

3.5 Interchangeability. All parts having the same manufacturer's part number shall be functionally and dimensionally interchangeable. The item identification and part number requirements of AD-1350 shall govern the manufacturer's part numbers and changes thereto.

3.6 Marking. Each RRHB shall be marked for identification in accordance with MIL-STD-130.

3.6.1 Identification plates. Each RRHB shall bear a metal plate in accordance with MIL-P-15024, Type A, G, or H on which a serial number, part number, contract number, and manufacturer's code are marked. The plate shall provide sufficient blank surface to permit the marking of symbols to record the number of inspection and overhaul cycles and accessory changes incorporated.

3.7 Workmanship. The workmanship displayed in fabrication and assembly of the RRHB shall be such as to assure ability of the RRHB to meet its performance requirements under all applicable environmental conditions. Unauthorized repair, welding, loose rivets, heavy burrs, indiscriminate placement of fasteners, and parts assembled by introduction of high stresses not prescribed in design, are typical signs of inferior workmanship. The standards of workmanship exhibited in any approved sample, subject to any qualification stated in the Government's notice of approval, shall be determinative of the requirements of the contract relative to workmanship insofar as not specifically covered by applicable specifications.

4. QUALITY ASSURANCE PROVISIONS

4.1 Responsibility for inspection. Unless otherwise specified in the contract or purchase order, the contractor is responsible for the performance of all inspection requirements (examinations and tests) as specified herein. Except as otherwise specified in the contract or purchase order, the contractor may use his own or any other facilities suitable for the performance of the inspection requirements specified herein, unless disapproved by the Government.

MIL-B-85110A(AS)

The Government reserves the right to perform any of the inspections set forth in this specification where such inspections are deemed necessary to ensure supplies and services conform to prescribed requirements. See Table II for the complete listing of all specified tests.

4.1.1 Responsibility for compliance. All items shall meet all requirements of section 3. The inspection set forth in this specification shall become a part of the contractor's overall inspection system or quality program. The absence of any inspection requirements in the specification shall not relieve the contractor of the responsibility of ensuring that all products or supplies submitted to the Government for acceptance comply with all requirements of the contract. Sampling inspection, as part of manufacturing operations, is an acceptable practice to ascertain conformance to requirements, however, this does not authorize submission of known defective material, either indicated or actual, nor does it commit the Government to accept defective material.

4.2 Number of test samples. A minimum of two samples shall be provided for the qualification tests of paragraph 4.3. Four additional samples shall be provided for demonstration of paragraph 4.6.

4.3 Qualification tests. The following qualification tests shall be conducted in accordance with the respective tests and sequence shown in Table III.

4.3.1 Examination. The RRHB shall be carefully examined for conformance with the applicable drawings and the requirements of this specification not covered by tests.

4.3.2 Environmental tests. The environmental testing of the RRHB shall be conducted on the sample designated and sequences shown in Table III.

4.3.2.1 Low pressure test. The low pressure test shall be performed in accordance with MIL-STD-810, Method 500.3, Procedure I, except that the steps shall be as follows:

- Step 1 The RRHB shall be examined per 4.3.1 and subjected to the actuation (see 4.3.4.1) and manual release (see 4.3.4.2) tests under the standard ambient conditions specified in MIL-STD-810. A record of specific pretest data shall be made to determine that the RRHB is within the requirements of 4.3.1, 4.3.4.1, and 4.3.4.2.
- Step 2 Place the test item in the chamber in a manner to simulate shipment by air. Decrease the chamber pressure to 144.1 mm of mercury (Hg) (5.56 inches of Hg or 40,000 feet above sea level) at a rate not to exceed 2000 feet per minute (fpm). Maintain this pressure for not less than one (1) hour.
- Step 3 Return the chamber to standard ambient conditions at a rate not to exceed 2000 fpm.
- Step 4 Remove the items from the test chamber and perform the examination (see 4.3.1), actuation (see 4.3.4.1), and manual release (see 4.3.4.2) tests on the test samples and obtain results in accordance with MIL-STD-810.

MIL-B-85110A(AS)

TABLE II. Specified tests.

Test	Reference paragraph
Qualification	4.3
Examination	4.3.1
Environmental	4.3.2
Low pressure	4.3.2.1
High temperature	4.3.2.2
Low temperature	4.3.2.3
Dust	4.3.2.4
Humidity	4.3.2.5
Salt fog	4.3.2.6
Vibration	4.3.2.7
Structural	4.3.3
Tensile	4.3.3.1
Bending	4.3.3.2
Overload	4.3.3.3
Loading rate	4.3.3.4
Endurance	4.3.3.5
Operating	4.3.4
Actuation	4.3.4.1
Manual release	4.3.4.2
Maintenance	4.4
Human factors	4.5
Demonstration on aircraft	4.6
Additional tests	4.7

MIL-B-85110A(AS)

TABLE III. Qualification test sequence.

Test	Reference paragraph	MIL-STD-810		Test sample	
		Method	Procedure	A	B
Examination	4.3.1			X	X
Environmental	4.3.2			X	
Low pressure	4.3.2.1	500.3	I	X	
High temperature	4.3.2.2	501.3	II	X	
Low temperature	4.3.2.3	502.3	I	X	
Dust	4.3.2.4	510.3	I	X	
Humidity	4.3.2.5	507.3	III	X	
Salt fog	4.3.2.6	509.3	I	X	
Vibration	4.3.2.7	514.4	Categories 4 & 5	X	
Structural	4.3.3				
Tensile	4.3.3.1			X	
Bending	4.3.3.2			X	
Overload	4.3.3.3			X	
Loading rate	4.3.3.4				X
Endurance	4.3.3.5				X
Operating	4.3.4			X	X
Actuation	4.3.4.1			X	X
Manual release	4.3.4.2			X	X

MIL-B-85110A(AS)

4.3.2.2 High temperature. The high temperature test shall be performed in accordance with MIL-STD-810, Method 501.3, Procedure II, except that the steps shall be as follows:

- Step 1 The RRHB shall be examined per 4.3.1 and subjected to the actuation (see 4.3.4.1) and manual release (see 4.3.4.2) tests under the standard ambient conditions specified in MIL-STD-810. A record of specific pretest data shall be made to determine that the RRHB is within the requirements of 4.3.1, 4.3.4.1 and 4.3.4.2.
- Step 2 Raise the internal chamber temperature to 66°C (150°F).
- Step 3 Maintain internal chamber temperature for 6 hours at 66°C (150°F).
- Step 4 Raise the internal chamber temperature to 71°C (160°F) within a time period of 1 hour and then maintain at that temperature for 4 additional hours.
- Step 5 Lower the internal chamber temperature to 66°C (150°F) within a time period of 1 hour.
- Step 6 Repeat steps 3, 4, and 5 two additional times (making a total of three 12 hour cycles.)
- Step 7 Adjust the internal chamber temperature to 71°C (160°F) and maintain until temperature stabilization of test item is reached.
- Step 8 Remove RRHB from chamber and install on a test fixture within a five minute time period. Perform the actuation (see 4.3.4.1) and manual release (see 4.3.4.2) tests, after which examine the test item per 4.3.1.
- Step 9 Return the RRHB to standard ambient conditions and stabilize.
- Step 10 Remove the RRHB from the test chamber and perform the examination (see 4.3.1), actuation (see 4.3.4.1), and manual release (see 4.3.4.2) tests on the test samples and obtain results in accordance with MIL-STD-810.

NOTE: The rate of temperature change (steps 2, 4 and 6) may be the maximum attainable by the chamber, but shall not exceed 10°C (18°F) per minute.

4.3.2.3 Low temperature. The low temperature test shall be performed in accordance with MIL-STD-810, Method 502.3, Procedure I, except that the steps shall be as follows:

MIL-B-85110A(AS)

- Step 1 The RRHB shall be examined per 4.3.1 and subjected to the actuation (see 4.3.4.1) and manual release (see 4.3.4.2) tests under the standard ambient conditions specified in MIL-STD-810. A record of specific pretest data shall be made to determine that the RRHB is within the requirements of 4.3.1, 4.3.4.1, and 4.3.4.2.
- Step 2 Lower the internal chamber temperature to the storage temperature -57°C (-70°F) and maintain for a period of 24 hours after stabilization.
- Step 3 Increase temperature to -40°C (-40°F) and stabilize RRHB temperature. Remove RRHB from chamber and install on a test fixture within a five minute time period, and perform the actuation (see 4.3.4.1) and manual release (see 4.3.4.2) tests, after which examine the test item per 4.3.1.

4.3.2.4 Dust. The material used in testing shall contain a mixture of 50% - 50% by weight of deck compound per MIL-D-23003 and MIL-STD-810 dust. The dust test shall be performed in accordance with MIL-STD-810, Method 510.3, Procedure I, except that the steps shall be as follows:

- Step 1 The RRHB shall be examined per 4.3.1 and subjected to the actuation (see 4.3.4.1) and manual release (see 4.3.4.2) tests under the standard ambient conditions specified in MIL-STD-810. A record of specific pretest data shall be made to determine that the RRHB is within the requirements of 4.3.1, 4.3.4.1 and 4.3.4.2.
- Step 2 Set the chamber controls to maintain an internal chamber temperature of 23°C (73°F) and a relative humidity of less than 22%. Adjust the air velocity to $1,750 \pm 250$ feet per minute. Adjust the dust feeder to control the dust concentration at 0.3 ± 0.2 grams per cubic foot. With the test item nonoperating, maintain these conditions for 6 hours.
- Step 3 Stop the dust feed and reduce the air velocity to 300 ± 200 feet per minute. Raise the internal chamber air temperature to 63°C (145°F). Hold these conditions 16 hours.
- Step 4 While holding chamber temperature at 63°C (145°F) adjust the air velocity to $1,750 \pm 250$ fpm. Adjust the dust feeder to control the dust concentration at 0.3 ± 0.2 grams per cubic foot. Unless otherwise specified, with the test item nonoperating, maintain these conditions for 6 hours.

MIL-B-85110A(AS)

- Step 5 Turn off all chamber controls and allow the test item to return to standard ambient conditions. Remove accumulated dust from the test item by brushing, wiping, or shaking, care being taken to avoid introduction of additional dust into the test item. Dust shall not be removed by either air blast or vacuum cleaning.
- Step 6 Remove RRHB from chamber and install on test fixture. Perform the actuation (see 4.3.4.1) and manual release (see 4.3.4.2) tests, after which examine the test item per 4.3.1. When inspecting per 4.3.1, test items containing bearings, grease seals, lubricants, shall be carefully examined for the presence of dust deposits.

4.3.2.5 Humidity. The humidity test shall be performed in accordance with MIL-STD-810, Method 507.3, Procedure III, except that the steps shall be as follows:

- Step 1 The RRHB shall be examined per 4.3.1 and subjected to the actuation (see 4.3.4.1) and manual release (see 4.3.4.2) tests under the standard ambient conditions specified in MIL-STD-810. A record of specific pretest data shall be made to determine that the RRHB is within the requirements of 4.3.1, 4.3.4.1, and 4.3.4.2.
- Step 2 Dry the test item at a temperature of not less than 40°C (104°F) nor more than 50°C (122°F) for not less than 2 hours.
- Step 3 Condition the test item at $25^{\circ} \pm 5^{\circ}\text{C}$ ($77^{\circ} \pm 9^{\circ}\text{F}$) and 50% relative humidity for 24 hours.
- Step 4 Subject the test item to five 24 hour cycles in accordance with Figure 507.3-3 of MIL-STD-810. Do not make the performance measurements at the times indicated on Figure 507.3-3. A 24-hour cycle consists of 16 hours at $60^{\circ} \pm 5^{\circ}\text{C}$ ($140^{\circ} \pm 9^{\circ}\text{F}$) and approximately 8 hours at $30^{\circ} \pm 5^{\circ}\text{C}$ ($86^{\circ} \pm 9^{\circ}\text{F}$) (includes transition times). The relative humidity shall be maintained at 95%, or greater, at both temperatures. Each transition time between $30^{\circ} \pm 5^{\circ}\text{C}$ ($86^{\circ} \pm 9^{\circ}\text{F}$) and $60^{\circ} \pm 5^{\circ}\text{C}$ ($140^{\circ} \pm 9^{\circ}\text{F}$) shall be not greater than 1 1/2 hours. The relative humidity during each transition need not be controlled. Approximately 2 hours after stabilization during the high temperature and low temperature portions of the first or second cycle, a sampling of the atmosphere in the chamber shall be made to determine that the conditions of temperature and relative humidity are uniform throughout the chamber. The RRHB

MIL-B-85110A(AS)

shall be removed from the chamber during the third cycle at $60^{\circ} \pm 5^{\circ}\text{C}$ ($140^{\circ} \pm 9^{\circ}\text{F}$) immediately prior to decreasing to $30^{\circ} \pm 5^{\circ}\text{C}$ ($86^{\circ} \pm 9^{\circ}\text{F}$) and installed on the test fixture within a 10 minute time period. Perform the actuation (see 4.3.4.1) and manual release (see 4.3.4.2) tests, after which examine the RRHB per 4.3.1. If the RRHB passes these inspections it shall be replaced in the chamber and subjected to the remaining 24 hour cycles in accordance with Figure 507.3-3.

Step 5 Condition the RRHB at $25^{\circ} \pm 5^{\circ}\text{C}$ ($77^{\circ} \pm 9^{\circ}\text{F}$) and $50 \pm 5\%$ relative humidity for not less than 12 hours nor more than 24 hours.

Step 6 Remove the RRHB from the chamber after the conditioning period and install on the test fixture within a 10 minute time period. Perform the actuation (see 4.3.4.1) and manual release (see 4.3.4.2) tests, after which examine per 4.3.1 and obtain results in accordance with MIL-STD-810.

4.3.2.6 Salt fog. The salt fog shall be performed in accordance with MIL-STD-810, Method 509.3, Procedure I, except that the performance test shall be as follows:

Performance test:

Step 1 The RRHB shall be examined per 4.3.1 and subjected to the actuation (see 4.3.4.1) and manual release (see 4.3.4.2) specified in MIL-STD-810. A record of specific pretest data shall be made to determine that the RRHB is within the requirements of 4.3.1, 4.3.4.1, and 4.3.4.2.

Step 2 The RRHB shall be placed in the test chamber in accordance with the general requirements of MIL-STD-810 and exposed to the salt fog for a period of 48 hours.

Step 3 Remove the RRHB and install on test fixture within a 10 minute time period. Perform the actuation (see 4.3.4.1) and manual release (see 4.3.4.2) tests, after which examine the test item per 4.3.1. These inspections shall be compared with those of step 1 and discrepancies noted.

Step 4 The RRHB shall then be inspected for corrosion in accordance with the general requirements of MIL-STD-810. If necessary to aid in the examination, a gentle wash in running water not warmer than 38°C (100°F) may be used.

MIL-B-85110A(AS)

- Step 5 Store the RRHB in an ambient atmosphere for 48 hours for drying.
- Step 6 At the end of the drying period the RRHB shall be subjected to the actuation (see 4.3.4.1) and manual release (see 4.3.4.2) tests under ambient conditions, after which it shall be examined per 4.3.1. These inspections shall be compared with those of step 1 and step 3.

4.3.2.7 Vibration test. The vibration tests shall be performed in accordance with MIL-STD-810, Method 514.4, equipment categories 4 and 5.

4.3.3 Structural tests. The following structural tests are required.

4.3.3.1 Tensile test. A special test sample RRHB, with the tensile control section omitted, shall be loaded to the maximum tensile load as specified in 3.4.4.1.

4.3.3.2 Bending test. The RRHB shall have a 400 pound static load applied at the midpoint normal to the longitudinal axis with the bar ends rigidly supported for three cycles. Upon completion of the bending load test, the examination (see 4.3.1), actuation (see 4.3.4.1), and manual release (see 4.3.4.2) tests shall be performed on the test samples.

4.3.3.3 Overload test. The RRHB/holdback fitting combination shall be loaded to failure with the RRHB in a locked condition to verify that the overload tensile control section fails within the limits of 3.4.3 (r).

4.3.3.4 Loading rate range. Prior to the start of the endurance test (see 4.3.3.5) the RRHB shall be tested for three cycles at a loading rate of 125,000 pounds/second and three cycles at a loading rate of 800,000 pounds/second. Release load shall be within the range specified in 3.4.4.3.

4.3.3.5 Endurance test. The RRHB shall be endurance tested for 2000 cycles including a scatter factor of two (or twice the lifetime specified in the applicable airplane detail specification) in accordance with (a), (b), and (c) using the loads of Figure 1.

- (a) A cycle shall consist of a manual hook-up, application of loads specified in Figure 1, release, and an impact with a steel deck. The release load shall be monitored throughout the test.
- (b) 10% of the endurance test shall be performed at the high ambient of 160°F (71°C) and 10% at the low ambient of -40°F (-40°C). See Figure 2 and Figure 3 for endurance test setup.
- (c) Twenty cycles at 160°F (71°C) shall be performed in rapid succession where the interval between cycles shall not exceed two minutes.
- (d) Calibration shall be permitted after each 100 cycles.

MIL-B-85110A(AS)

4.3.4 Operating tests. The following operating tests are required.

4.3.4.1 Actuation. The RRHB shall be subjected to ten cycles of applied rated load and release. All ten cycles shall operate within the specified range. The unit shall reset itself after each release.

4.3.4.2 Manual release. The RRHB shall be subjected to five manual release cycles at the maximum override release load of 3.4.3(k). The unit shall reset itself after each release.

4.4 Maintainability. The contractor shall conduct maintainability tests in accordance with a maintainability program acceptable to the acquiring activity (see 6.3).

4.5 Human factors engineering verification. The contractor shall conduct human factors engineering verification tests in accordance with a program acceptable to the acquiring activity. The tests shall be performed to demonstrate compliance with MIL-STD-1472. The effect of degraded inputs and degraded performance shall be determined (see 6.3).

4.6 Demonstration on aircraft. A test program shall be developed by the contractor for testing the RRHB using a catapult and launching an aircraft. The program shall monitor and record all parameters of the design and operational requirements as specified herein and on the applicable drawings.

4.7 Additional tests. The contractor shall propose additional qualification and acceptance inspections as necessary for the assurance design adequacy and continued production reliability based upon the peculiarities of the specific design.

5. PACKAGING

This section is not applicable to this specification.

6. NOTES

(This section contains information of a general or explanatory nature that may be helpful, but is not mandatory.)

6.1 Intended use. The requirements of this specification are intended for use in the design, construction, performance and test of a repeatable release holdback bar (RRHB) for use on carrier-type aircraft.

6.2 Acquisition requirements. Acquisition documents should specify the following:

- a. Title, number, and date of this specification.
- b. Applicable aircraft detail specification.
- c. Issue of DODISS to be cited in the solicitation, and if required, the specific issue of individual documents referenced (see 2.1.1 and 2.2).

MIL-B-85110A(AS)

6.3 Consideration of data requirements. The following data requirements should be considered when this specification is applied on a contract. The applicable Data Item Descriptions (DID's) should be reviewed in conjunction with the specific acquisition to ensure that only essential data are requested/provided and that the DID's are tailored to reflect the requirements of the specific acquisition. To ensure correct contractual application of the data requirements, a Contract Data Requirements List (DD Form 1423) must be prepared to obtain the data, except where DOD FAR Supplement 27.475-1 exempts the requirement for a DD Form 1423.

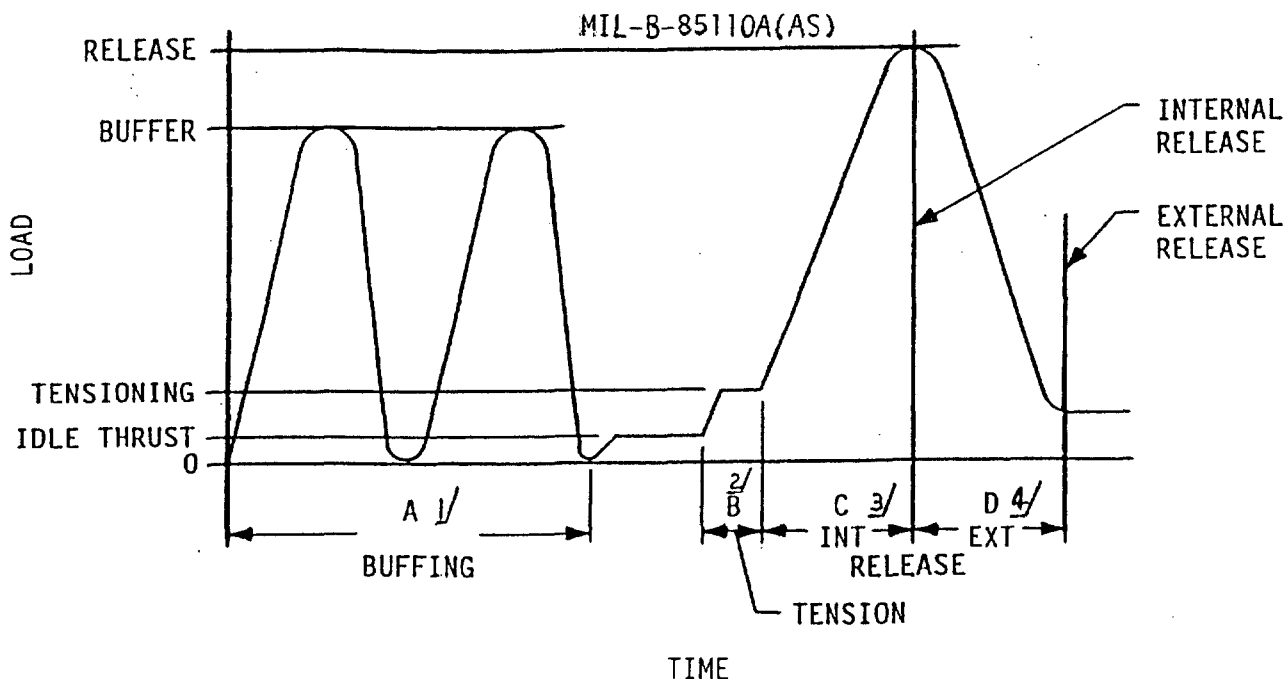
<u>Reference Paragraph</u>	<u>DID Number</u>	<u>DID Title</u>	<u>Suggested Tailoring</u>
4.4	DI-MNTY-80822	Maintainability program plan	---
4.5	DI-HFAC-80740	Human engineering program plan	---

The above DID's were those cleared as of the date of this specification. The current issue of DOD 5010.12-L, Acquisition Management Systems and Data Requirements Control List (AMSDL), must be researched to ensure that only current, cleared DID's are cited on the DD Form 1423.

6.4 Subject term (key word) listing.

Human factors engineering verification
Qualitative
Quantitative
Storage life

6.5 Changes from previous issue. Marginal notations are not used in this revision to identify changes with respect to the previous issue due to the extensiveness of the changes.



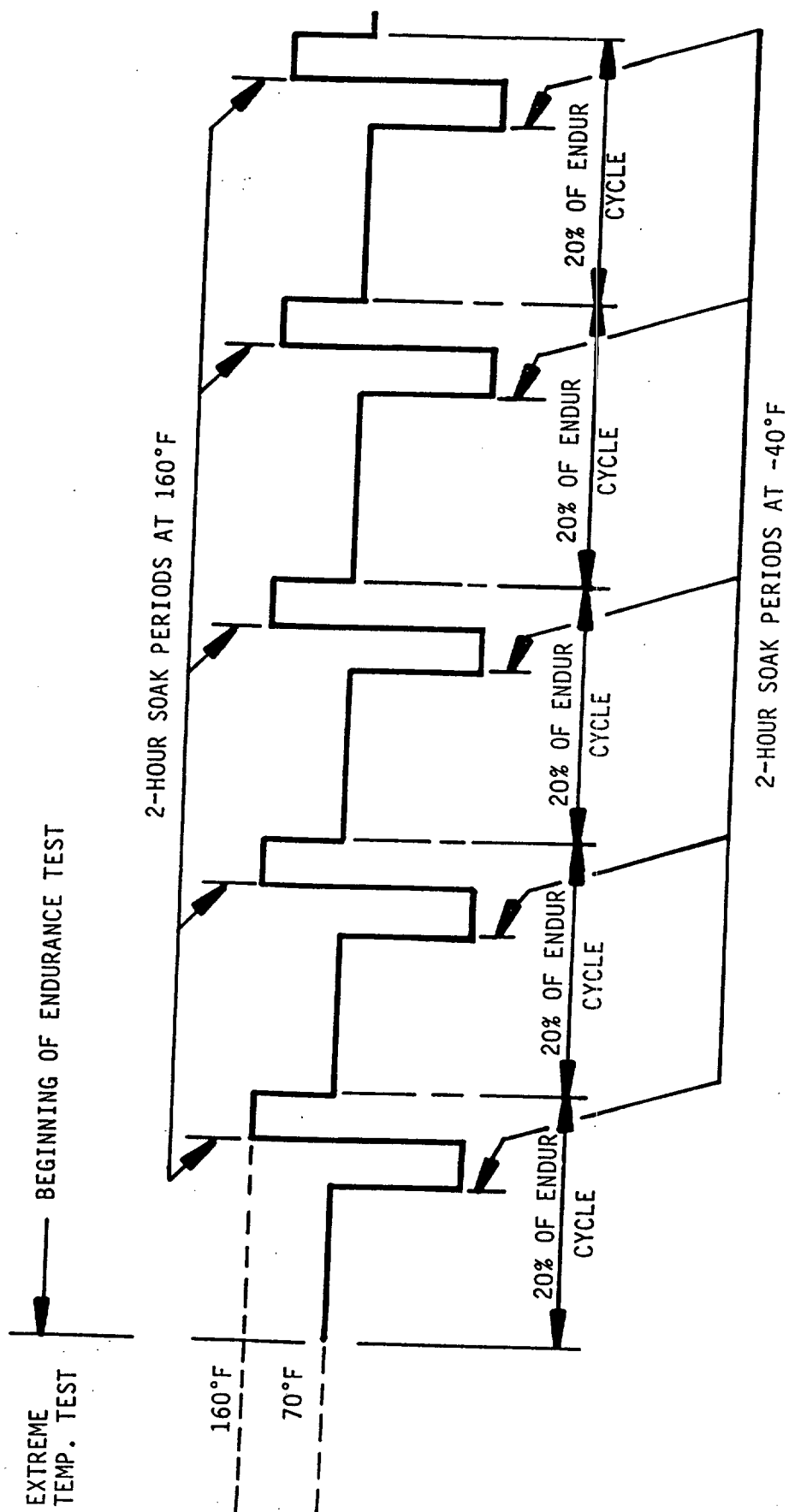
- 1/ Time A Two cycles of buffer loading. Each cycle is from zero to .8Rcos A (see figure 3 for angle A) applied at the aircraft holdback fitting. Load drops to zero pounds at end of each buffer cycle.
- 2/ Time B Load representing tensioning condition. Loading rate and time held at this load not critical. Tensioning load is equal to catapult tensioning of 5500 pounds plus "T" as specified in MIL-A-8863.
- 3/ Time C Load to internal release. A load P is applied to cause the RRHB to release at load of R as specified in MIL-A-8863. The rate of loading for P shall be as specified in 3.4.3(g).
- 4/ Time D Load drops to approximately 1000 pounds at which point external release takes place and RRHB falls to steel plate (plate size shall not be less than 60 inches in length, 24 inches in width, and 1 inch in thickness).

NOTES:

1. A total of 1000 (or as specified in the applicable airplane detail specification) catapult launches is required not including scatter factor. All of these launches follow the schedule above exactly.

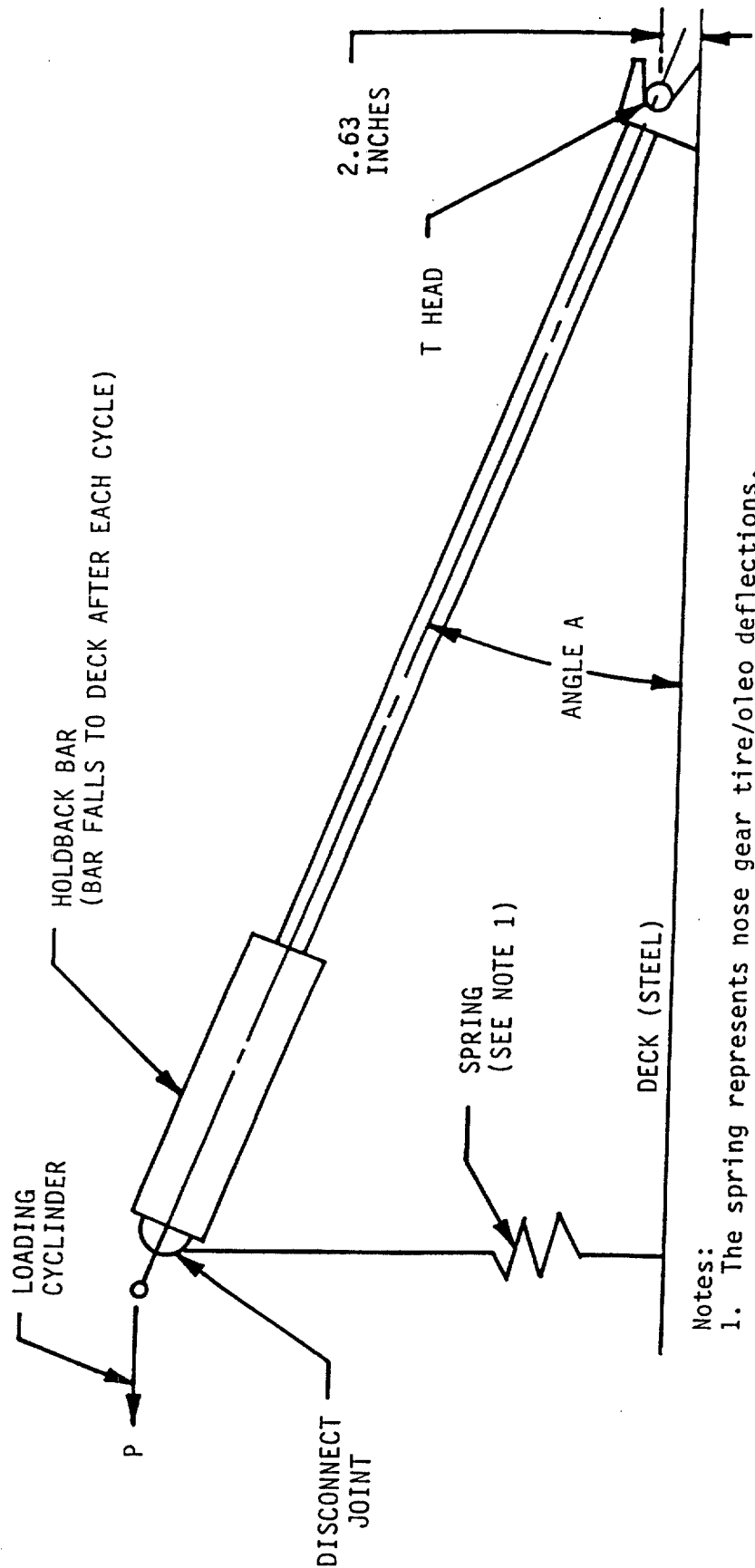
FIGURE 1. Schematic of holdback bar loading during launch cycle.
(See Figure 3 for test set-up and geometry).

MIT-B-85110(AS)

FIGURE 2. Endurance test temperature cycling schematic graph.

MIL-B-85110(AS)

Condition	Load P	Angle A + 30'
Hook-up Buffing Tensioned Release	See FIGURE 1	Variable to suit design geometry



Notes:

1. The spring represents nose gear tire/oleo deflections.

FIGURE 3. Hookup for endurance and fatigue cycle.

MIL-B-85110A(AS)

NUMERICAL INDEX

<u>PARAGRAPH</u>		<u>PAGE</u>
1.	SCOPE	1
1.1	Scope	1
2.	APPLICABLE DOCUMENTS	1
2.1	Government documents	1
2.1.1	Specifications, standards, and handbooks	1
2.1.2	Other Government documents, drawings and publications	4
2.2	Non-Government publications	4
2.3	Order of precedence	5
3.	REQUIREMENTS	5
3.1	Selection of materials and standard parts	5
3.1.1	Materials	5
3.1.1.1	Metal parts	5
3.1.1.1.1	Heat treatment	5
3.1.1.1.2	Magnetic particle inspection	5
3.1.1.1.3	Castings	5
3.1.1.1.4	Forgings	6
3.1.1.1.5	Magnesium and magnesium alloys	6
3.1.1.2	Non-metallic components	6
3.1.1.3	Lubrication	6
3.1.1.4	Decalcomanias	6
3.1.1.5	Potting compounds	6
3.1.1.6	Fungus-proof materials	6
3.1.2	Corrosion protection	6
3.1.2.1	Finishes	6
3.1.2.2	Anodizing	7
3.1.2.2.1	Chemical surface treatment	7
3.1.2.3	Metal coatings	7
3.2	Selection of specifications and standards	7
3.3	Drawings	8
3.4	Design and construction	8
3.4.1	Dimensions	8
3.4.2	Weight	8
3.4.3	Operating characteristics	8
3.4.4	Loads	10
3.4.4.1	Ultimate	10
3.4.4.2	Endurance	10
3.4.4.3	Release	10
3.4.4.4	Overload	10
3.4.5	Environmental	10
3.4.5.1	Temperature	10
3.4.5.2	Low pressure	10
3.4.5.3	Dust	11
3.4.5.4	Humidity	11
3.4.5.5	Salt fog	11
3.4.5.6	Vibration	11
3.4.5.7	Rough usage	11

MIL-B-85110A(AS)

NUMERICAL INDEX

<u>PARAGRAPH</u>		<u>PAGE</u>
3.4.6	Reliability	11
3.4.7	Maintainability	11
3.4.7.1	Quantitative	11
3.4.7.1.1	Minimum cycles between overhaul	11
3.4.7.1.2	Organizational level	12
3.4.7.1.2.1	Scheduled	12
3.4.7.1.3	Intermediate level	12
3.4.7.1.4	Depot level	12
3.4.7.1.5	Support equipment	12
3.4.7.2	Qualitative	12
3.4.7.2.1	Preventive maintenance	12
3.4.7.3	Personnel skill level	12
3.4.8	Storage life	12
3.4.9	Human engineering	13
3.5	Interchangeability	13
3.6	Marking	13
3.6.1	Identification plates	13
3.7	Workmanship	13
4.	QUALITY ASSURANCE PROVISIONS	13
4.1	Responsibility for inspection	13
4.1.1	Responsibility for compliance	14
4.2	Number of test samples	14
4.3	Qualification tests	14
4.3.1	Examination	14
4.3.2	Environmental tests	14
4.3.2.1	Low pressure test	14
4.3.2.2	High temperature	17
4.3.2.3	Low temperature	17
4.3.2.4	Dust	18
4.3.2.5	Humidity	19
4.3.2.6	Salt fog	20
4.3.2.7	Vibration test	21
4.3.3	Structural tests	21
4.3.3.1	Tensile test	21
4.3.3.2	Bending test	21
4.3.3.3	Overload test	21
4.3.3.4	Loading rate range	21
4.3.3.5	Endurance test	21
4.3.4	Operating tests	22
4.3.4.1	Actuation	22
4.3.4.2	Manual release	22
4.4	Maintainability	22
4.5	Human factors engineering verification	22
4.6	Demonstration on aircraft	22
4.7	Additional tests	22

MIL-B-85110A(AS)

NUMERICAL INDEX

<u>PARAGRAPH</u>		<u>PAGE</u>
5.	PACKAGING	22
6.	NOTES	22
6.1	Intended use	22
6.2	Acquisition requirements	22
6.3	Consideration of data requirements	23
6.4	Subject term (key word) listing	23
6.5	Changes from previous issue	23
 <u>TABLE</u>		
I.	Metal coatings	7
II.	Specified tests	15
III.	Qualification test sequence	16
 <u>FIGURE</u>		
1	Schematic of holdback bar loading during launch cycle	24
2	Endurance test temperature cycling schematic graph	25
3	Hookup for endurance and fatigue cycle	26

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MIL-B-85110A(AS)

2. DOCUMENT DATE (YYMMDD)

30 August 1991

3. DOCUMENT TITLE

BAR, REPEATABLE RELEASE HOLDBACK, AIRCRAFT LAUNCHING, GENERAL DESIGN SPECIFICATIONS FOR

4. NATURE OF CHANGE (Identify paragraph number and include proposed rewrite, if possible. Attach extra sheets as needed.)

5. REASON FOR RECOMMENDATION

6. SUBMITTER

a. NAME (Last, First, Middle Initial)

b. ORGANIZATION

c. ADDRESS (Include Zip Code)

d. TELEPHONE (Include Area Code)

(1) Commercial

(2) AUTOVON

(if applicable)

7. DATE SUBMITTED
(YYMMDD)

8. PREPARING ACTIVITY

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Commanding Officer
NAEC (SESD) Code 53

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